

PRIOR TO ISSUANCE OF GRADING PERMITS

The following shall be completed:

No comments.

PRIOR TO ISSUANCE OF BUILDING PERMITS

The following shall be completed:

Improvements: The applicant shall design their street improvement plans to include the following:

OASIS ROAD

- The driveway on Oasis Road.
 - The driveway on Oasis Road shall be right in and right out only with a "R3-2" sign. The sign shall be placed within the property line at the driveway and the owner is responsible for maintaining the sign.

The total fair share contribution for this project is required based on the traffic report dated 10/14/2025 from TJW Engineering. The fair share breakdown for these improvements is shown below:

INTERSECTION	ESTIMATED COST	FAIR SHARE PERCENTAGE	ESTIMATED CONTRIBUTION
Mountain Road at Route 138			
Intersection Total	\$600,000 to \$800,000	7.62%	\$45,720 to \$60,960
		Total Fair Share	\$45,720 to \$60,960

- The total fair share contribution will be based on the fair share percentages listed above and the estimated construction costs at the time of application for a building permit and shall be paid to the Department of Public Works - Traffic Division. At the present time, the estimated cost is \$45,720 to \$60,960 pending final estimated construction cost approval from Caltrans. This amount will be adjusted to reflect actual construction costs incurred, if available, or will be adjusted to account for future construction costs using the Caltrans Construction Cost Index.

PRIOR TO OCCUPANCY/FINAL INSPECTION

The following shall be completed:

- This project falls within the High Desert Local Area Transportation Facilities Fee Plan. This fee shall be paid by a cashier's check to the Department of Public Works Business Office. These fees are subject to change. Based on the ITE Trip Generation Manual (11th Edition) and a 5,637 sq. ft. convenience store with 28 fueling positions as shown on the site plan, this project generates approximately 2,569 vehicle trips on a weekday. This fee is \$193.55 per trip multiplied by the number of vehicle trips (2,569) and multiplied by an induced trip adjustment factor of 20% as shown in the fee plan. Therefore, the total estimated Local Transportation Fees for this project is \$99,445.99. The current High Desert Local Area Transportation Facilities plan can be found at the following website: <https://dpw.sbcounty.gov/transportation/transportation-planning/>

AP/SJ

Cc: File

Commissioner Coaching the Applicant

Slowik (1:19:30) so the so the so okay so the follow-on question to that is that with this particular project then would under current circumstances or under improved circumstances meaning with regard to any improvements required by this project would trucks be able to leave the site and go west on Buckthorn meaning making a right-hand turn and going bucked on Buckthorn to the west for any reason

Dice- no

Slowik- no okay (1:20:06) so okay now with that with that so with regard to that driveway number one I could well we could assume then that based on this those circumstances of that roadway that all of the trucks exiting the site from driveway one would be going left and heading towards Oasis

Dice- correct

Slowik- and then going back out onto the going back to the highway okay okay so so if that's the case and again sticking with driveway one there is there would be or would not be trucks accessing the site making a right into driveway one what would be or would not be

Dice (1:20:50) it's designed where a truck could go in okay however with our signage we want the trucks to circulate in driveway two, go in and fill and then come out driveway one

Slowik- correct I followed that okay and again I can see on the site plan that unless I'm off a little bit here on my viewing that driveway two does look a little bit more angled as opposed to perpendicular for that purpose of allowing the trucks to obviously access the site it looks like it's more angular as opposed to completely perpendicular is it

(The site plan referenced is not the approved plan; it was introduced in this hearing. Slowick is making a suggestion to Deis about the design, leading Deis to make a conclusion.)

Dice- correct

Slowik- and that's the intention of that

Dice- correct

Slowik- and and so based on again the circumstances I've asked about Buckthorne Road I guess similarly there wouldn't be any reason for trucks that are leaving driveway two to make a right-hand turn and go west on Buckthorne just like driveway one there wouldn't be any reason for that correct

Dice- correct

(Slowick is making a suggestion to Deis about the design, leading Deis to make a conclusion. Slowick is not an engineer/designer.)

Slowik- okay but would trucks be able to exit in driveway two

Dice-No. because of the way that the truck will flow it would be there is enough room on the exit side of the high flow that a truck could turn around and do that but we will have significant striping on the

ground at the driveway entrances stop bar stop everything else there and I have done this at many other sites as well that we can put additional signage saying exit out driveway no you turn

(1:22:30) Slowik- so that and so again adding all this together so the intention is the for the truck traffic coming in from Oasis to Buckthorne accessing driveway two onto the site and then exiting driveway one back onto Buckthorne back out to Oasis back out to the highway

Slowick is making a suggestion to Deis about the design, leading Deis to make a conclusion.)

(1:22:50) Dice-yes that's that's yeah it separates the autos from the trucks and it gives a lot better flow for the trucks right through

Post Office Traffic Flow

Slowik- but and again in this case it's gonna that's gonna involve some signage and striping and other informational directional yeah matters to the to the to the drivers mm-hmm and okay so let's see it's the one slide the one slide that showed the post office I think that was on the appellants presentation and it was the one what was proposed post office traffic circulation that particular slide but can we bring that up there this one proposed the other that's current

(1:24:00) *Dice*-this is current

Slowik- yeah well let's go to the proposed post office traffic there we go I basically and maybe again this maybe this this one's most appropriate I think for the staff to address basically couldn't tell from the image here because the question has to do with the left-hand turn pocket lane going to the into the post office and again it's cut off at the northern end of this this diagram here is that my question basically then to staff is how long in distance is that left-hand turn turn lane pocket because again it's cut off here on the image and so forth

Kevin Deis:

(38:48) Now, with regard to the post office, and I don't have the post office picture up here. **We did speak with the post office. They are amenable to reversing the flow so that when vehicles come up Oasis, they get to Buckthorn.** There'll be a stop sign at Buckthorn. They can take the left into the post office, circulate around, come out below Buckthorn, take a right, there'll be a stop sign there. So, Buckthorn and Oasis will be fully signalized with stop signs. So, it will be controlled.

(We interviewed the Postmistress in Pinon Hills. She didn't have any information on any approval by the Postal Service to change the flow of traffic in the parking lot. She contacted her superiors, and they said the same thing.)

Signalizing Oasis Road & Buckthorne Road Intersection

Kevin Deis:

(38:48) So, Buckthorn and Oasis will be fully signalized with stop signs. So, it will be controlled. I think it was mentioned that Oasis is 55 miles an hour. I do not believe cars are gonna go from the highway up to Buckthorn and reach 55 miles an hour. That's 400 feet, 500 feet at most, I think. So, I don't think with regard to excessive speed will be an issue on that one.

(County Response: The traffic study evaluates the intersections for signal and stop-sign warrants. The findings in the traffic study did not support a 4-way stop at the intersection of Oasis Rd and Buckthorne Rd. Proposed signing and striping is reviewed as part of the street improvement plan review)

Unapproved Site Plan Introduced

Slowick:

I just want to make sure what I didn't we didn't have something duplicative over. Let's see here. **Now then if I'm sorry we can go back to the site plan slide.** That'll help since I lost it on my computer. Okay. Okay. I have heard and absorbed the entire series of presentations regarding the driveways and so we'll for clarity we'll keep my question relative to the same number sequence you gave one two and three. So let's begin with driveway number three which is a one-off of Oasis. And again I'm boiling my questions down to simplicity so I'm able to completely understand and with clarity and follow along. With regard to that driveway entrance will trucks be able to enter that driveway entrance?

(The site plan referenced above is not the approved site plan. It was introduced in this hearing. Reference Exhibit 11. Therefore, all the information that follows is not accurate or approved.)

(1:18:01) *Deis:*

It is designed so trucks can enter however with our signage we are we want the trucks to go down to Buckthorn and then go in.

Slowick:

Okay. Before I ask a question the next question about the separate driveway or staff can enter or address this or anyone who can address it. So Buckthorn Road currently to the west it doesn't it doesn't go through to another street to the west?

Dice (1:18:32) A dirt road yes it does **it goes all the way over through the mountain road** I believe. I believe that that there is some dirt that people do continue to travel down. Now as a public street or not? I don't know.

(Buckthorne Road does not go through to Mountain Road. There are barriers, private properties, and a Community Center blocking thru access.)

Slowik (1:18:48) Okay the reason and as you can see the reason for the question is on some of the information the map information and presentation shared with us there's a note on there relative to Buckthorn Road there's a note on there that says no outlet so that's the reason for my question to get clarity there's a dirt road that goes to the west but the question then is for my own clarity is does it go all the way through to another street? So it sounds like there's a discrepancy as to whether or not it does that's what

Dice (1:19:15) it's a dedicated roadway on I would say that the county's roadway plans okay now how the the residents utilize that road okay I'm not sure okay okay

Mr. Weldy's Comments and Tone

Weldy: The I'm trying to decide how deep to go the challenge of this is quite simply this this is it appears to be a conforming use and so when we say in our zoning we're going to go through and we're going to do zoning maps we're going to create a general plan and this is what we see in the future that read that we have there is an open for business sign for him that read there says we're looking for commercial in this area we'll allow it it's a conforming use so when you and **and mr. Hale a shout out to you a fascinating seminar on traffic I I appreciate it it's interesting but the design flaws or not in how their traffic flows isn't a reason to deny a project for land use it's a reason to say hey you're gonna have people driving over your curbs and and you're gonna be unhappy with the way this works because your driveways aren't wide enough or whatever that is it'll solve itself** *but this is really not a design criteria as much as it is this a conforming land use are they allowed to do what they want to do on this land.*

(Mr. Hale has 32 years of experience in Road Design with San Bernardino County Department of Public Works. To demean him with a sarcastic statement about a “fascinating seminar” was disrespectful and not befitting the serious subject at hand.) Mr. Weldy is saying that development is more important than safety.

Mr. Weldy's general disrespect for the concerned citizens of Pinon Hills and San Bernardino County is not appreciated. This Maverik issue determines how our community will proceed in the coming years. If it is approved, our residential area will become a major industrial corridor that will lower property values for homeowners and ruin our rural way of life. His sarcastic tone was hurtful and unnecessary.

Weldy: I live in Apple Valley and **so to whoever it was that said well you wouldn't vote for this if it was on the other side of your house if it was zoned commercial on the other side of my house** I would vote for it because it's a land use that's entitled it goes with the property it has value and so when you say that could have been a rural market that is owned by my neighbor next door and and they only use crops that grow in their backyard conforming use it's still allowed the same way that **this unhappy use for you** is I think allowed so part of the question you're asking when you say you can't vote for this the the interest of the community is more important than the interest of Utah business I understand that I get that except **that's not a reason to say you can't develop this property for a consistent zoning use so no no there's not a dialogue**

APN: 3068-231-68
ZONING: GENERAL COMMERCIAL

815 HIGHWAY 138 #1-2
PINON HILLS, CA 92372-9280
APN: 3068-231-40-0
HEMINGWAY FAMILY TRUST /97
ZONING: GENERAL COMMERCIAL

PROPOSED 44' EASEMENT

EXISTING PROPERTY LINE

BUCKTHORNE RD PINON HILLS, CA
APN: 3067-051-29-0000
PLS & ASSOCIATES LLC
ZONING: GENERAL COMMERCIAL

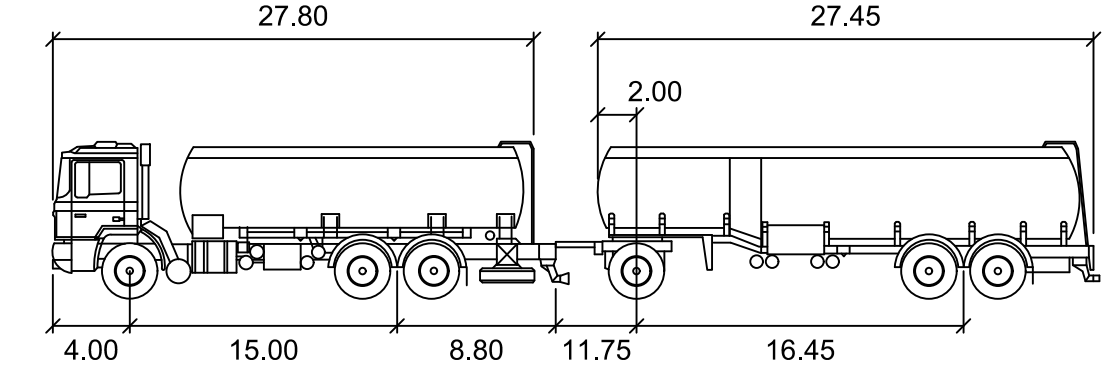
777 SMOKE TREE RD
APN: 3067-051-10-0000
MEYER CARROLLEE M TRUST 8-17-09
ZONING: GENERAL COMMERCIAL

LOT 8
TRACT MAP NO. 3319
M.B. 4427

833 BUCKTHORNE RD
PINON HILLS, CA 92372
APN: 3067-051-13-00
ISAIAS A PALMA / DIANA L PALMA
ZONING: SINGLE FAMILY RESIDENTIAL

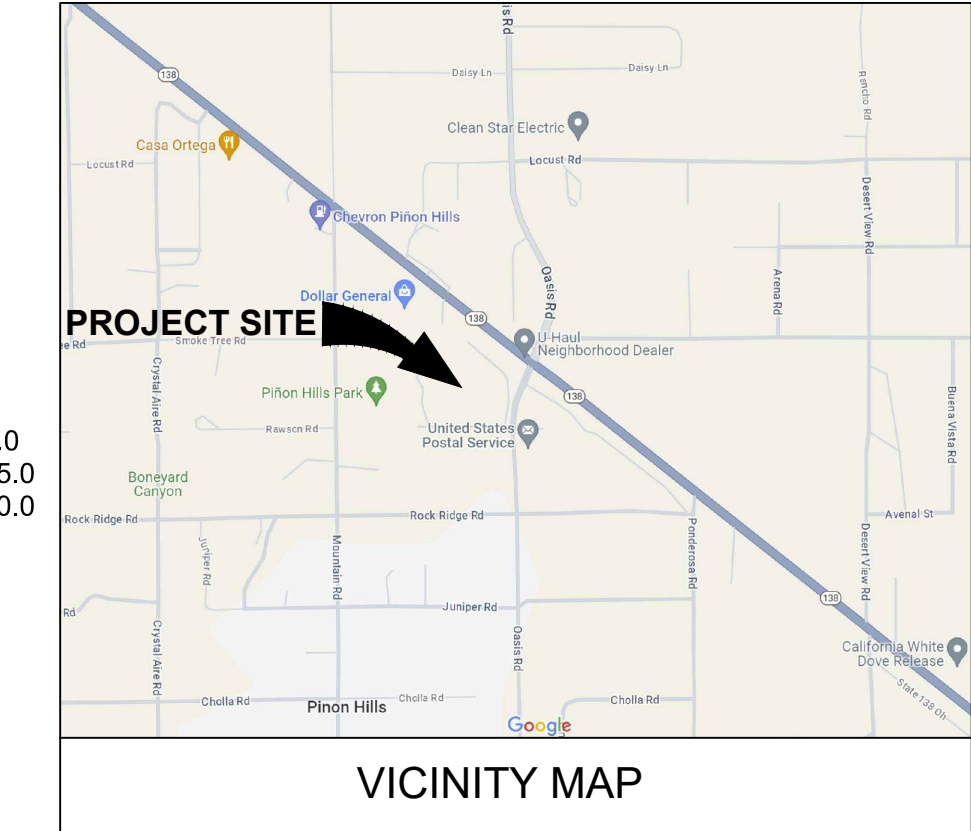
VACANT LAND
10344 OASIS RD
PINON HILLS, CA 92372

POST OFFICE
APN: 3067-091-13
ZONING: GENERAL COMMERCIAL



Custom
First Unit Width : 8.00
Trailer Width : 8.00
First Unit Track : 7.70
Trailer Track : 7.70

Lock to Lock Time : 6.0
Steering Angle : 35.0
Articulating Angle : 70.0

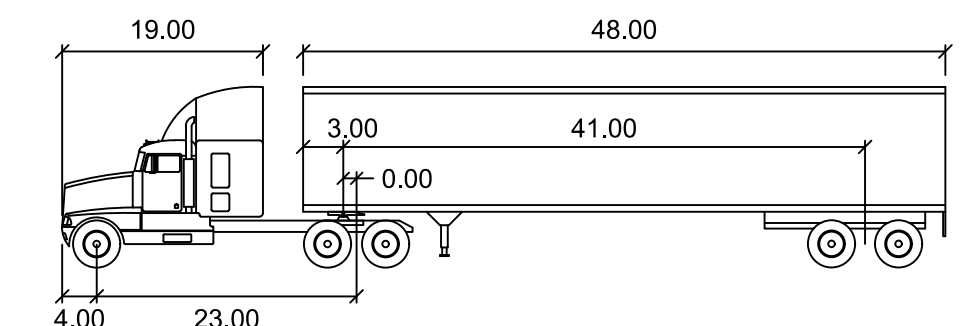


SITE LEGEND

- EXISTING PROPERTY BOUNDARY LINE
- EXISTING ADJOINING PROPERTY LINE
- EXISTING UTILITY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED PROPERTY LINE
- PROPOSED EASEMENT
- PROPOSED RIGHT OF WAY
- PROPOSED STORM DRAIN LINE
- EXISTING EDGE OF PAVEMENT
- PROPOSED CURB AND GUTTER
- PROPOSED CURB
- PROPOSED ROLLER CURB
- PROPOSED BUILDING
- PROPOSED SIDEWALK
- PROPOSED CONCRETE PAVEMENT
- PROPOSED LIMIT OF DISTURBANCE
- PROPOSED WATER STRUCTURES
- PROPOSED STORM STRUCTURES
- EXISTING ELECTRIC STRUCTURE
- PROPOSED PARKING COUNT
- EXISTING TREE
- EXISTING SHRUB
- EXISTING JOSHUA TREE
- EXISTING WATER STRUCTURE
- EXISTING ELECTRIC STRUCTURE
- EXISTING GAS MAIN
- EXISTING WATER MAIN
- EXISTING UNDERGROUND ELECTRIC
- EXISTING TELEPHONE
- EXISTING OVERHEAD WIRES

ZONING DATA
ZONED "CG" GENERAL COMMERCIAL

ITEM	EXISTING / REQUIRED	PROPOSED
PARCEL ID	APN 3067-051-29-0000	APN 3067-051-29-0000
LAND USE	GENERAL COMMERCIAL	GENERAL COMMERCIAL
BUILDING HEIGHT (FT)	ALLOWED 35 FT	10 FT (1 STORY)
BUILDING AREA (SF)	VACANT	5,637
FLOOR AREA RATIO (FAR)	25% MAXIMUM	1.5%
FRONT YARD (FT)	25	120
SIDE YARD (FT)	25	25
REAR YARD (FT)	10	70
PARKING SPACES	23 STALLS	42 STALLS
ADA PARKING SPACES	2 STALLS	2 STALLS
TOTAL PARCEL AREA	8.78 AC ±	8.78 AC ±
LANDSCAPING (SF)	0	222,06 ±
PERCENTAGE OF NET SITE AREA	0	58%
TRASH ENCLOSURE SIZING	N/A	12'-8"W x 34'-8"L



STAA Design Vehicle (56 FT RADIUS)

Tractor Width : 8.50
Trailer Width : 8.50
Tractor Track : 8.50
Trailer Track : 8.50

Lock to Lock Time : 6.0
Steering Angle : 26.1
Articulating Angle : 70.0

CORE STATES GROUP

4240 E. Juniper Street, Suite 402
Ontario, CA 91761
Phone (909) 467-9907
srulic@core-states.com

DOCUMENTS PREPARED BY CORE STATES, INC. INCLUDING THIS DOCUMENT ARE TO BE USED ONLY FOR THE SPECIFIC PROJECT AND SPECIFIC USE FOR WHICH THEY WERE INTENDED. ANY EXTENSION OF USE TO ANY OTHER PROJECTS, BY OWNER OR BY ANY OTHER PARTY, WITHOUT THE EXPRESSED WRITTEN CONSENT OF CORE STATES, INC. IS DONE UNLAWFULLY AND AT THE USER'S OWN RISK. IT IS USED IN A WAY OTHER THAN THAT SPECIFICALLY INTENDED. USER WILL HOLD CORE STATES, INC. HARMLESS FROM ALL CLAIMS AND LOSSES.

MAVERIK

PREPARED FOR:

811

Know what's below. Call before you dig.

REVISIONS

REV	DATE	COMMENT	BY

DOCUMENT
CONCEPTUAL SITE PLAN FOR MAVERIK PROGRAM

SITE LOCATION
10450 OASIS ROAD
PINON HILLS, CA 92372

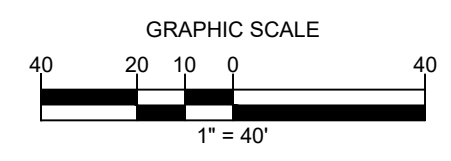
ENGINEER SEAL

NOT FOR CONSTRUCTION

SHEET TITLE
CONCEPTUAL SITE PLAN

JOB #: MAV.30727.CIVIL
DATE: 11/10/2025
SCALE:
DRAWN BY: JOC
CHECKED BY: KCH

SHEET NO.
C1.0



A
B
C
D
E
F
G
H
I

6" CURB AND GUTTER

NOTES:
 1. CURB AND GUTTER SHALL BE CONSTRUCTED MONOLITHICALLY OF MINOR CONCRETE.
 2. WIDTHS OF STANDARD STREET SECTIONS SHOWN ON PLANS ARE TO CURB LINE UNLESS OTHERWISE INDICATED.
 3. WEAKENED PLANE JOINTS (8"x12" DEEP) SHALL BE CONSTRUCTED AT TEN FOOT (10') INTERVALS, EXCEPT THAT THE INTERVAL SHALL BE VARIED TO ALLOW MATCHING OF JOINTS IN ADJACENT EXISTING IMPROVEMENTS.
 4. CURING COMPOUND SHALL BE SPRAYED UNIFORMLY ON EXPOSED SURFACES.
 5. WHEN CURB AND GUTTER IS PLACED BY AN EXTRUSION MACHINE, MINOR FINISHING MAY BE DONE TO PROVIDE AN ACCEPTABLE FINISH AND THE WEAKENED PLANE JOINTS MAY BE SAWCUT.
 6. PEJ FILLER SHALL BE APPLIED IN THE WHOLE CROSS SECTION OF THE CURB AND GUTTER.
 7. WHEN IN FRONT OF A CURB RAMP, USE GUTTER PAN TRANSITION ON THE CURRENT CALTRANS STANDARD PLAN 488A.

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 116

COMMERCIAL DRIVEWAY APPROACH WITH CURB RETURNS

NOTES:
 1. FOR APPLICABLE NOTES AND DRIVEWAY REQUIREMENTS SEE STANDARD 130.
 2. SURFACING SHALL BE PORTLAND CEMENT CONCRETE (MINOR CONCRETE) 6" THICK AND HAVE A BROOM FINISH.
 3. THE CURB RETURN RADIUS AND/OR MAXIMUM WIDTH OF DRIVEWAY SHALL BE INCREASED TO ACCOMMODATE A TRUCK TURNING PATH.

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 129B

ASPHALT CONCRETE DIKE

NOTES:
 1. DIKE SHALL BE CONSTRUCTED OF TYPE A ASPHALT CONCRETE.
 2. PAINT BINDER SHALL BE PLACED ON EXISTING ASPHALT CONCRETE PAVEMENT PRIOR TO THE INSTALLATION OF THE DIKE.

6" ASPHALT DIKE	0.5'	0.5'	1.08'	0.5'
8" ASPHALT DIKE	0.5'	0.67'	1.08'	0.67'

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 117

MAJOR HIGHWAY

NOTES:
 1. STRUCTURAL SECTION OF ROADWAY SHALL BE DETERMINED FROM SOIL TESTS AND SO INDICATED ON CONSTRUCTION PLANS.
 2. DRAINAGE FACILITIES SHALL BE PROVIDED TO DETERMINED RAISED MEDIAN AREAS.
 3. TEN FEET (10') SHOULDER AREAS MAY BE DESIGNATED AS BIKE LANES AND EMERGENCY PARKING ONLY.
 4. IRRIGATION SYSTEM IN MEDIAN AREA SHALL BE DRIP OR BUBBLE SYSTEM.

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 101

LOCAL STREET

NOTES:
 1. STRUCTURAL SECTION OF ROADWAY SHALL BE DETERMINED FROM SOILS TESTS, AND SO INDICATED ON CONSTRUCTION PLANS.
 2. MINIMUM DESIGN PAVING THICKNESS SHALL BE 0.20' ASPHALT CONCRETE.
 3. CONSTRUCTION OUTSIDE RIGHT OF WAY WILL REQUIRE SLOPE EASEMENTS.
 4. WHEN PREPARING SUBGRADE FOR PAVING, CENTERLINE CROWN ON THE "LEVEL SECTION" SHALL BE RELOCATED EITHER LEFT OR RIGHT 0.50' TO MATCH CROWN BREAK IN PAVING MACHINE.

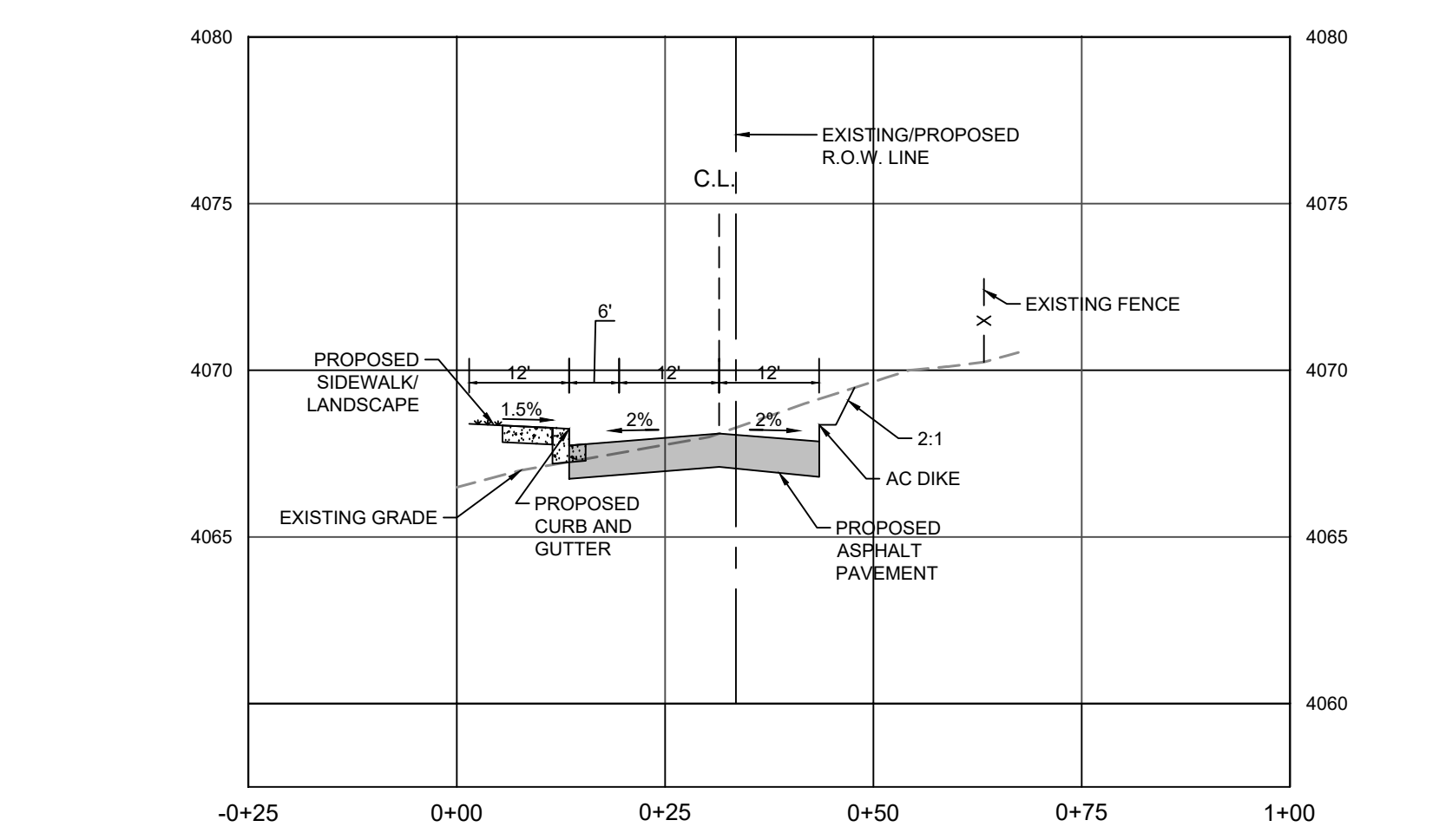
TILT					
8" CURB	LEVEL	0.00'	0.33'	0.22'	0.33'
8" CURB	TILT	0.44'	0.66'	0.50'	0.33'
6" CURB	LEVEL	0.00'	0.16'	0.05'	0.16'
6" CURB	TILT	0.44'	0.49'	0.33'	0.16'

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 104

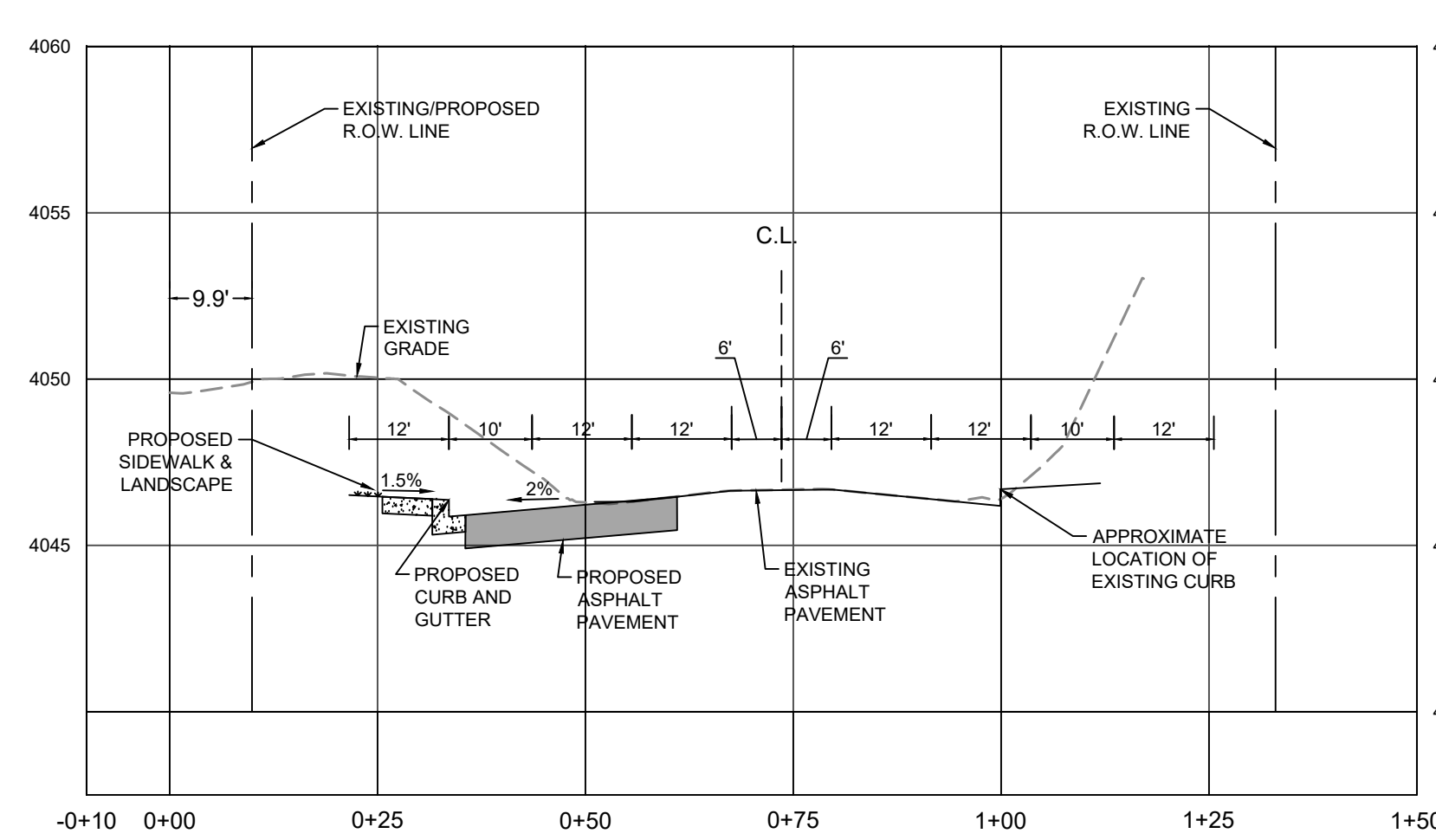
SIDEWALK

NOTES:
 1. TYPE "C" SIDEWALKS ADJACENT TO CURB SHALL ONLY BE USED ON LOCAL AND COLLECTOR STREETS AND ONLY UPON APPROVAL OF THE TRANSPORTATION DEPARTMENT.
 2. SIDEWALK SHALL BE CONSTRUCTED OF 4" THICK MINOR CONCRETE.
 3. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED ON TEN FEET (10') SPACING.
 4. SCORING SIDEWALK WILL BE PERMITTED.
 5. IN EXPANSIVE SOIL AREAS, REFER TO SECTION 73 OF THE CALTRANS STANDARD SPECIFICATIONS.

SAN BERNARDINO COUNTY DEPARTMENT OF PUBLIC WORKS
 BRENDON P. BIGGS, PE
 DIRECTOR OF PUBLIC WORKS/ROAD COMMISSIONER
 109



CROSS SECTION A-A' - BUCKTHORNE R



CROSS SECTION B-B' - OASIS RD

CORE STATES GROUP

4240 E. Juniper Street, Suite 402
 Ontario, CA 91761
 Phone (909) 467-9907
 srulic@core-states.com

DOCUMENTS PREPARED BY CORE STATES, INC. INCLUDING THIS DOCUMENT ARE TO BE USED ONLY FOR THE SPECIFIC PROJECT AND SPECIFIC USE FOR WHICH THEY WERE INTENDED. ANY EXTENSION OF USE TO ANY OTHER PROJECTS, BY OWNER OR BY ANY OTHER PARTY, WITHOUT THE EXPRESSED WRITTEN CONSENT OF CORE STATES, INC. IS DONE UNLAWFULLY AND AT THE USER'S OWN RISK. IT IS USED IN A WAY OTHER THAN THAT SPECIFICALLY INTENDED. USER WILL HOLD CORE STATES, INC. HARMLESS FROM ALL CLAIMS AND LOSSES.

PREPARED FOR:

MAVERIK

Know what's below. Call before you dig.

811

REVISIONS

REV	DATE	COMMENT	BY

DOCUMENT
 CONCEPTUAL SITE PLAN FOR MAVERIK PROGRAM

SITE LOCATION
 10450 OASIS ROAD
 PINON HILLS, CA 92372

ENGINEER SEAL

NOT FOR CONSTRUCTION

SHEET TITLE
 CONCEPTUAL CROSS SECTION AND STANDARD DETAILS

JOB #: MAV.36727.CIVIL
 DATE: 11/10/2025
 SCALE:
 DRAWN BY: JOC
 CHECKED BY: KCH

SHEET NO.
C2.0